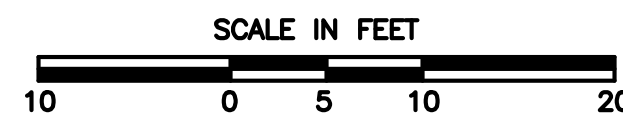
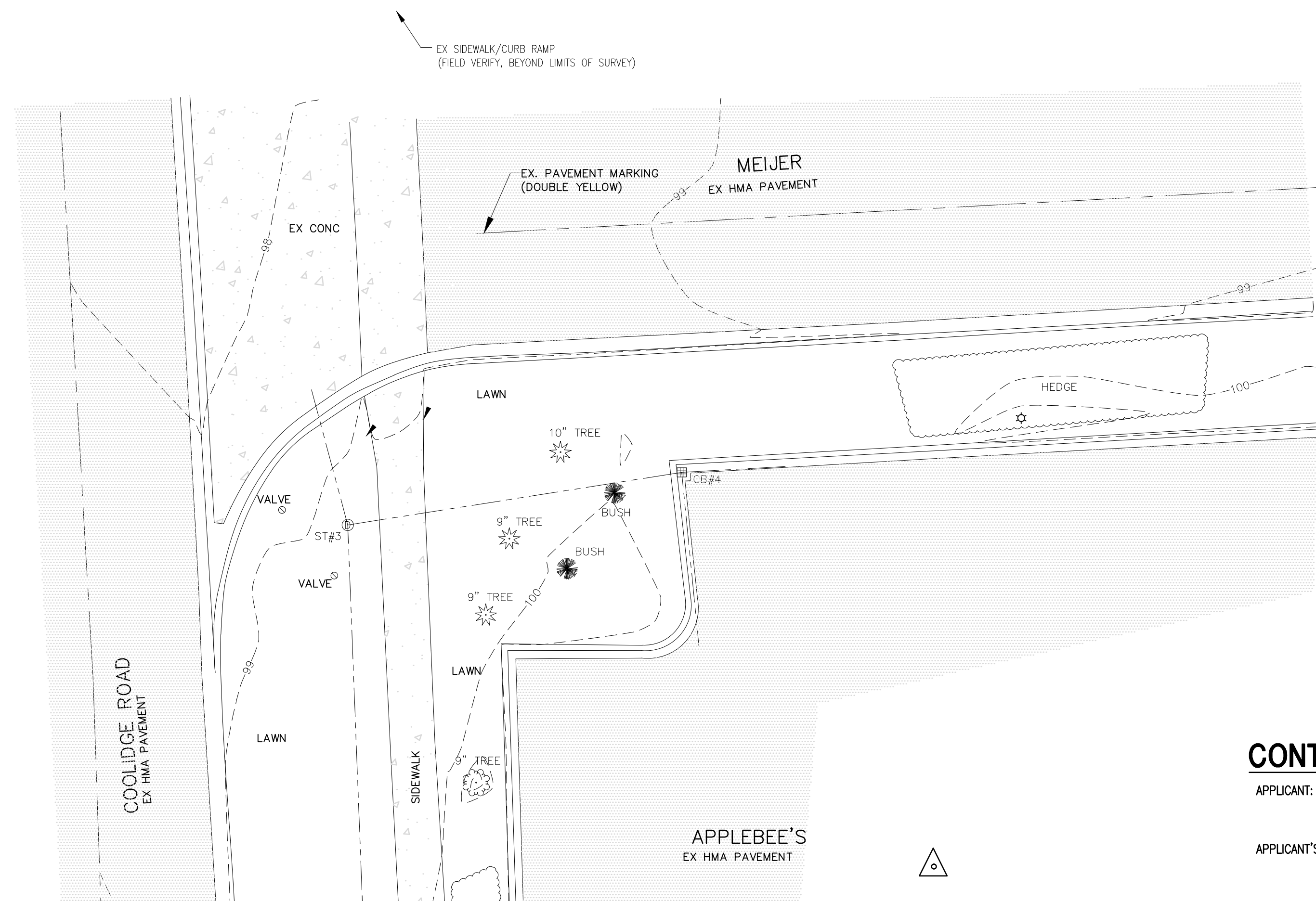


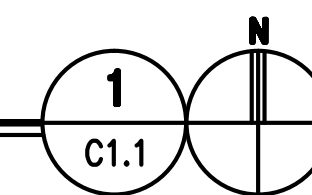
GENERAL NOTES: TEMPORARY LANE CLOSURE

1. PRIOR TO OR UPON COMMENCEMENT OF WORK IN THE COOLIDGE ROAD RIGHT-OF-WAY, CONTRACTOR SHALL TEMPORARILY CLOSE ONLY THE RIGHT (EASTERNMOST) LANE OF COOLIDGE ROAD TO CONSTRUCT THE NEW DRIVE APPROACH.
2. TEMPORARY TRAFFIC CONTROL AND SIGNAGE SHALL BE FURNISHED, OPERATED, AND MAINTAINED BY THE CONTRACTOR THROUGHOUT CONSTRUCTION AND AS DIRECTED BY THE CITY OF EAST LANSING, ENGINEERING DEPARTMENT.
3. LANE CLOSURE AND TRAFFIC SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION IN ACCORDANCE WITH MDOT, MAINTAINING TRAFFIC TYPICAL - "M0400a TYPICAL TEMPORARY TRAFFIC CONTROL FOR CLOSING ONE LANE OF A FIVE-LANE UNDIVIDED ROADWAY AND MAINTAINING TWO THROUGH LANES IN EACH DIRECTION, NO SPEED REDUCTION".
4. CONTRACTOR IS RESPONSIBLE FOR PROVIDING ADDITIONAL BARRICADES AND TEMPORARY SIGNAGE TO MAINTAIN TRAFFIC EAST OF THE DRIVE APPROACH ON THE SERVICE DRIVE.



EXISTING CONDITIONS PLAN

SCALE: 1"=10'



CONTACT INFORMATION

APPLICANT: CAPITAL AREA TRANSPORTATION AUTHORITY
4615 TRANTER STREET
LANSING, MI 48910

APPLICANT'S REPRESENTATIVE: MR. BARRY FLOWERS
PHONE: (517) 394-1100

ENGINEER: DLZ MICHIGAN, INC.
1425 KEYSTONE AVE.
LANSING, MI 48911
PH: (517) 393-6800
FAX: (517) 272-7390

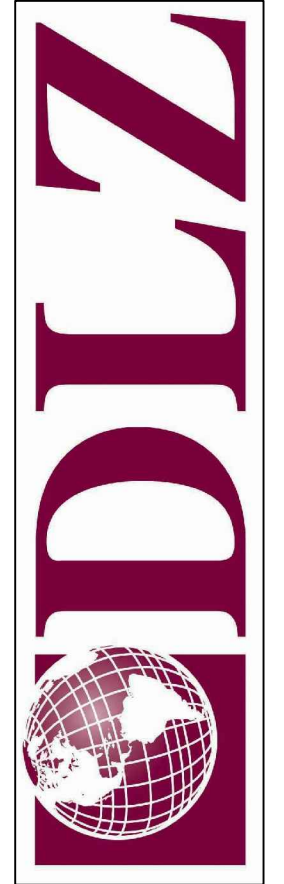
PROJECT MANAGER: MR. STEVE METZER
EMAIL: smetzer@dlz.com

EXISTING STRUCTURE INVENTORY

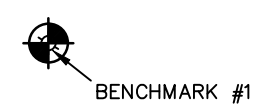
- STORM MANHOLE #1
RIM - 100.51
20" RCP NORTH - 94.25
15" RCP SOUTH - 94.34
12" RCP S30°W - 94.69
- CATCH BASIN #2
INLET - 99.42
12" RCP N30°E - 95.10
12" RCP WEST - 95.17
- STORM MANHOLE #3
RIM - 99.29
24" RCP N15°W - 93.14
14" PVC EAST - 93.39
20" RCP SOUTH - 93.94
- CATCH BASIN #4
INLET - 99.18
14" PVC WEST - 93.69
12" PVC EAST - 94.13
12" PVC SOUTH - 93.90

GENERAL NOTES:

- A. EXISTING SITE IMPROVEMENTS SHOWN HEREIN HAVE BEEN IDENTIFIED FOR GENERAL INFORMATION. UNLESS OTHERWISE NOTED, IT IS THE INTENT THAT ALL EXISTING IMPROVEMENTS WITHIN THE DEMOLITION LIMITS NOT INDICATED TO REMAIN, OR OTHERWISE PROTECTED, BE DEMOLISHED AND REMOVED FROM THE SITE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY AND ACCOUNT FOR ALL EXISTING IMPROVEMENTS TO BE REMOVED AND INCLUDE ALL SUCH ITEMS IN THEIR RESPECTIVE BASE BID.
- B. CONTRACTOR SHALL VISIT SITE PRIOR TO BIDDING TO BECOME FAMILIAR WITH EXISTING SITE AND ABOVE GROUND CONDITIONS TO BE REMOVED, ALTERED, OR PROTECTED.
- C. UTILITIES SHOWN ARE APPROXIMATE. FIELD VERIFY LOCATIONS OF ALL UTILITIES SHOWN AND NOT SHOWN ON DRAWING PRIOR TO CONSTRUCTION.
- D. DEMOLITION LIMITS SHOWN ARE APPROXIMATE. ADJUSTMENTS TO THE LIMITS INCIDENTAL TO DEMOLITION ACTIVITIES MAY BE PERMITTED WITH APPROVAL OF THE ENGINEER.
- E. SAW CUT EX. CURB AND GUTTER, CONCRETE AND HMA PAVEMENT AS REQUIRED TO INSTALL IMPROVEMENTS. PROTECT ADJOINING PAVEMENT TO REMAIN. SAW CUT CONCRETE AT NEAREST JOINT.
- F. EXISTING IMPROVEMENTS TO REMAIN WHICH ARE DAMAGED OR DESTROYED AS A RESULT OF CONTRACTOR ACTIVITIES SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- G. CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY TRAFFIC MAINTENANCE PLANS, RIGHT-OF-WAY PERMIT AND COORDINATION WITH CITY OF EAST LANSING ENGINEERING REGARDING THIS WORK.
- H. CONSTRUCTION STAGING. CONTRACTOR WILL COORDINATE STAGING AREAS WITH CATA & CONFORM WITH MEIJER STORE MANAGER PRIOR TO STARTING CONSTRUCTION.
- I. CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING & MAINTAINING THEIR OWN HORIZONTAL & VERTICAL CONTROL POINTS, BENCHMARKS, ETC. CONTRACTOR IS RESPONSIBLE FOR PROVIDING CONSTRUCTION STAGING AND FIELD LAYOUT. TOPOGRAPHIC SURVEY HAS BEEN PREPARED FOR THIS WORK ONLY IN PROXIMITY OF THE SOUTH CURB LINE.
- J. THE CONTRACTOR SHALL PERFORM SWEEPING OF PROJECT SITE AND COOLIDGE ROAD AS NEEDED TO REMOVE ANY SEDIMENT TRACKED OFF SITE.



DLZ MICHIGAN, INC.



REMOVAL LEGEND

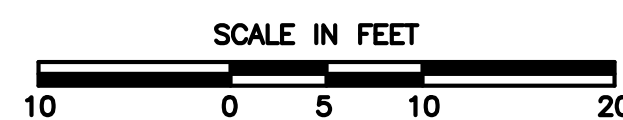
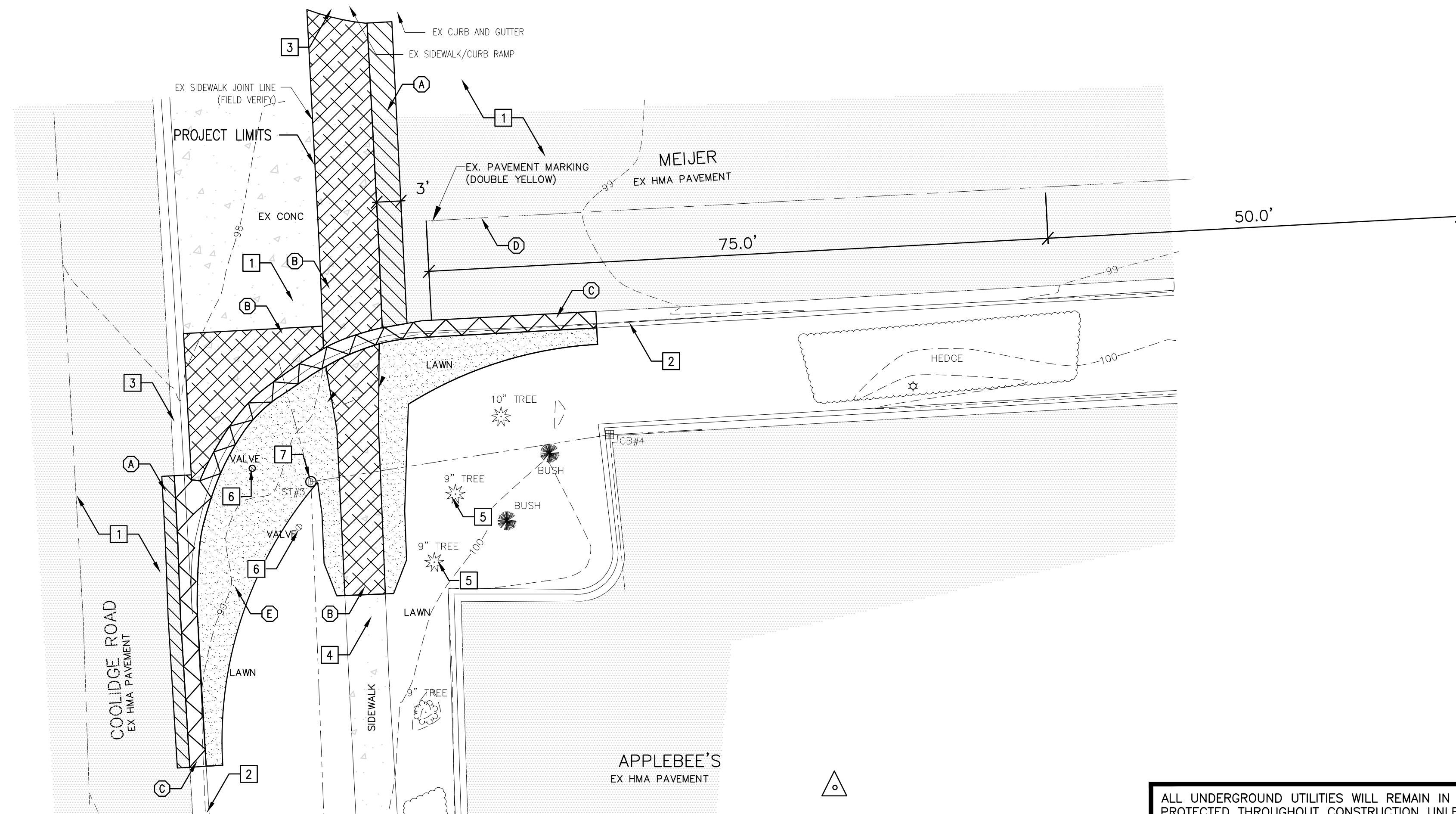
- REMOVE EX. LAWN & TOPSOIL
- SAWCUT FULL DEPTH & REMOVE EX. ASPHALT PAVEMENT
- REMOVE EX. CONCRETE PAVEMENT
- REMOVE EX. CONCRETE CURB & GUTTER

SITE REMOVAL KEY

- (A) REMOVE EX. ASPHALT COMPLETE
- (B) REMOVE EX. CONCRETE PAVEMENT
- (C) REMOVE EX. CONCRETE CURB AND GUTTER (REMOVE AT NEAREST JOINT)
- (D) REMOVE EX. PAVEMENT MARKING (DOUBLE YELLOW LINE)
- (E) REMOVE LAWN AND STRIP TOPSOIL AS NEEDED TO CONSTRUCT IMPROVEMENTS AND RESTORE ADJACENT LAWN AREA

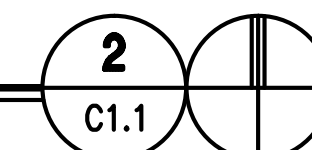
SITE PROTECTION AND RELOCATION KEY

- 1 EXISTING PAVEMENT TO REMAIN
- 2 EXISTING CURB TO REMAIN
- 3 EXISTING GUTTER PAN TO REMAIN
- 4 EXISTING WALK TO REMAIN
- 5 EXISTING TREE TO REMAIN
- 6 EXISTING VALVE BOX TO REMAIN (PROTECT THROUGHOUT CONSTRUCTION)
- 7 EXISTING MH (STORM) TO REMAIN (PROTECT THROUGHOUT CONSTRUCTION)



REMOVAL PLAN

SCALE: 1"=10'



ALL UNDERGROUND UTILITIES WILL REMAIN IN PLACE AND BE PROTECTED THROUGHOUT CONSTRUCTION UNLESS OTHERWISE NOTED ON THE PLANS. THE CONTRACTOR WILL PROVIDE NOTICE TO ALLOW ALL UTILITIES TO LOCATE THEIR FACILITIES PRIOR TO THE PERFORMANCE OF WORK. FOR PROTECTION OF UNDERGROUND UTILITIES, THE CONTRACTOR WILL CONTACT "MISS DIG" A MINIMUM OF 3-WORKING DAYS EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS PRIOR TO EXCAVATING IN THE VICINITY OF UTILITY LINES.

LEGEND

- = ASPHALT
- = CONCRETE
- = EXISTING SPOT ELEVATION
- = EXISTING CONTOUR ELEVATION
- = SANITARY SEWER
- = STORM SEWER
- = WATER LINE
- = GAS LINE
- = SANITARY MANHOLE
- = DRAINAGE MANHOLE
- = ELECTRIC MANHOLE
- = TELEPHONE MANHOLE
- = CATCHBASIN
- = CLEANOUT
- = FIRE HYDRANT
- = VALVE
- = UTILITY POLE
- = LIGHT POLE
- = UTILITY PEDESTAL
- = DECIDUOUS TREE
- = CONIFEROUS TREE

BENCHMARKS

BENCHMARK #1: ELEVATION: 102.46
CHISELED SQUARE ON SOUTH SIDE OF CONCRETE LIGHT POLE BASE IN NORTHWEST QUAD OF NORTH DRIVE TO KROGER OFF COOLIDGE ROAD

BENCHMARK #2: ELEVATION: 105.85
TOP OF BIG ARM OF FIRE HYDRANT SOUTH SIDE OF DRIVE BETWEEN BANK OF AMERICA AND APPLEBEES ON EAST SIDE OF COOLIDGE ROAD

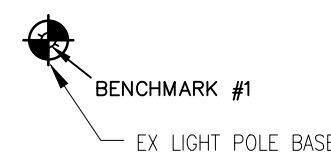


Know what's below.
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DRAWN: RMS
DESIGNED: RMS
APPROVED: SOM
DATE: JUNE 26, 2012
PROJECT NUMBER
0941-6302-00

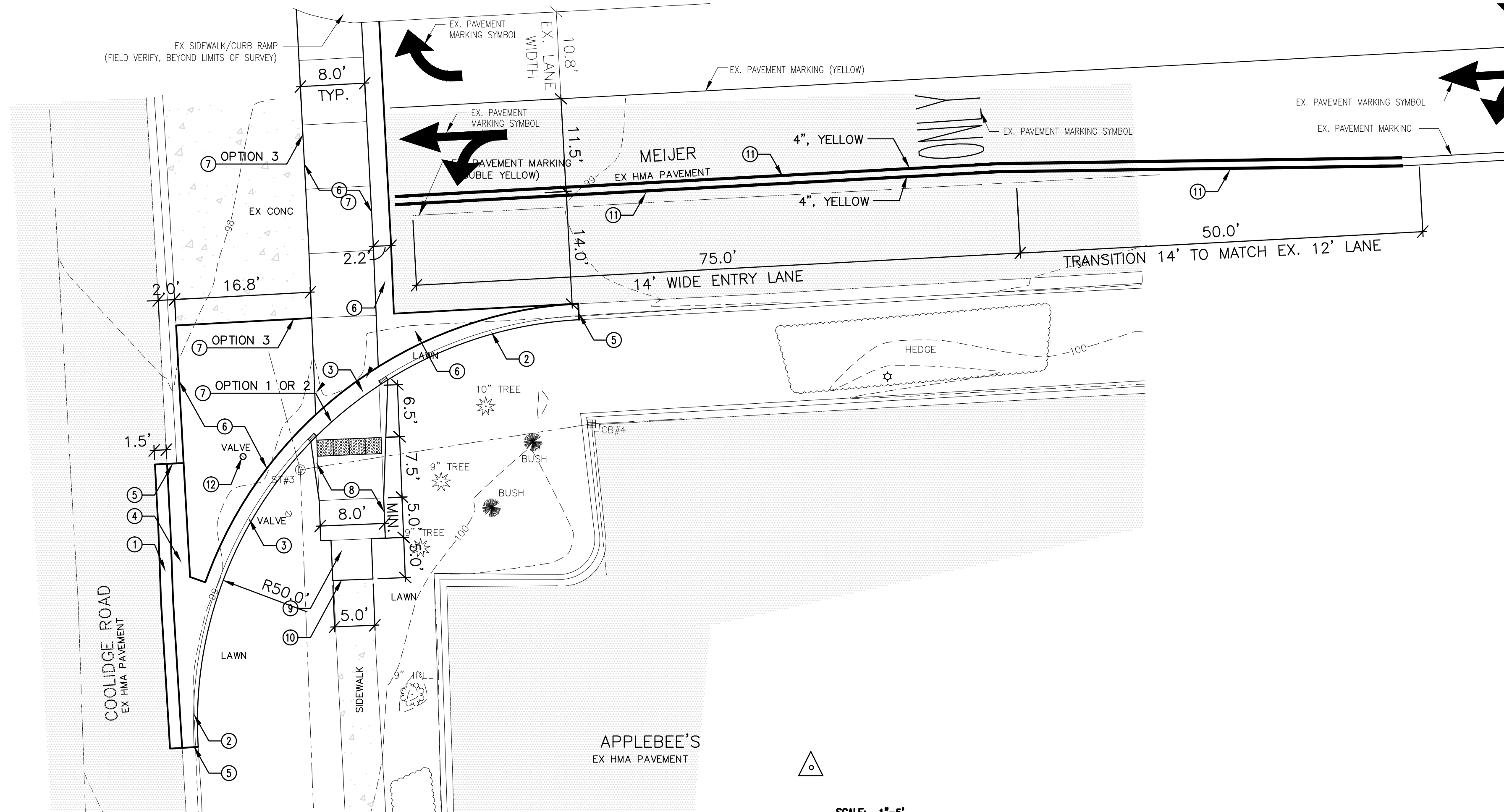
MICHIGAN
EAST LANSING
CAPITAL AREA TRANSPORTATION AUTHORITY
MODIFICATION TO EX. BOARDING CENTER AT LAKE LANSING MEIJER
EXISTING CONDITIONS & REMOVAL PLAN

DRAWING NUMBER
C1.1
SITE/CIVIL



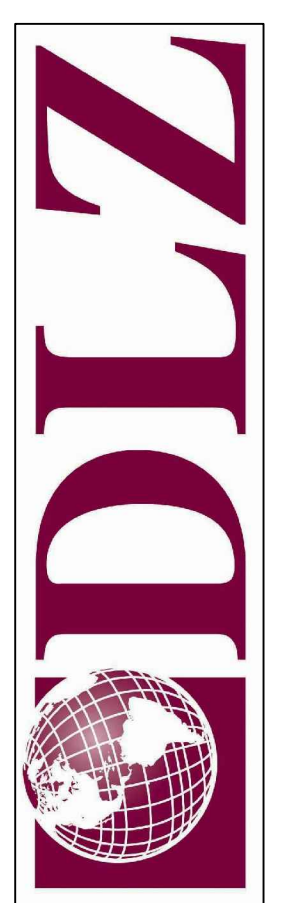
SITE IMPROVEMENT KEY

SYMBOL	REFERENCE NOTES	DETAIL
①	HOT MIX ASPHALT PAVEMENT, BY CONTRACTOR (MDOT 13A)	1
②	STANDARD CURB AND GUTTER SECTION	2
③	PITCH OUT CURB AND GUTTER SECTION	3
④	VALLEY GUTTER PLAN AND SECTION	4
⑤	CURB EXPANSION JOINT	5
⑥	CONCRETE DRIVE APPROACH	6
⑦	CONSTRUCTION JOINTS FOR 4"-6" THICKNESS	7
⑧	SIDEWALK RAMP, PERPENDICULAR	8
⑨	CONCRETE WALK AND JOINT	9
⑩	EXPANSION JOINT SECTION	10
⑪	PAVEMENT MARKING, WATERBORNE, 4 INCH, YELLOW	11
⑫	ADJUST VALVE BOX	12
⑬	EXPANSION JOINT SECTION	13

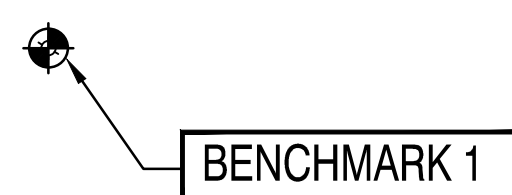


GENERAL NOTES

- ALL AREAS DISTURBED OUTSIDE THE PROJECT LIMITS SHALL BE RESTORED TO A CONDITION EQUAL TO OR BETTER THAN EXISTING PRIOR TO CONSTRUCTION.
 - CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY TRAFFIC MAINTENANCE PLANS, RIGHT-OF-WAY PERMIT AND COORDINATION WITH CITY OF EAST LANSING ENGINEERING REGARDING THIS WORK.
 - CONSTRUCTION STAGING. CONTRACTOR WILL COORDINATE STAGING AREAS WITH CATA & CONFIRM WITH MEIJER STORE MANAGER PRIOR TO STARTING CONSTRUCTION.
 - DIMENSIONS ARE TAKEN FROM THE BACK OF CURBS AND WALKS, UNLESS OTHERWISE NOTED.
 - ALL SPOT GRADES SHOWN ON THE PLANS ARE FINISH GRADES.
 - CONTRACTOR SHALL MATCH ALL EXISTING GRADES AT THE PROJECT LIMITS, TYPICAL.
 - PROPOSED GRADES AND SLOPES SHALL MATCH EXISTING GRADES AND SLOPES AT PROJECT LIMITS OR AS SHOWN ON DRAWINGS. WHERE INTERSECTING SLOPE ELEVATIONS VARY, PROVIDE SMOOTH TRANSITIONAL EDGE.
 - TRANSITIONS FROM PROPOSED SIDEWALKS AND PAVEMENTS SHALL BE UNIFORM AND SMOOTH WITHOUT ABRUPT CHANGES IN GRADE OR ALIGNMENT.
 - PROPOSED FINISHED GRADES SHALL PROVIDE FOR POSITIVE DRAINAGE TO A DRAINAGE STRUCTURE, IF PRESENT, OR MATCH EXISTING GRADES. INFORM ARCHITECT OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION.
 - CONTRACTOR MAY ADJUST PLAN GRADES AS NEEDED TO FACILITATE MATCHING EXISTING PAVEMENT AND LAWN GRADES TO PROVIDE SURFACE DRAINAGE AND PREVENT PONDING STORM WATER.
 - CONTRACTOR SHALL FILL LOW/DEPRESSIONAL GRADES WHICH MAY OCCUR THROUGH REQUIRED DEMOLITION, AS WELL AS, ANY AREA WITHIN THE PROJECT LIMITS, SO AS TO PROVIDE CONSTANT UNIFORM SLOPES.
 - GRADE AND CONSTRUCT ALL SIDEWALKS AND WALKS, REGARDLESS OF MATERIAL, TO A CROSS SLOPE OF 1.5% WITH A CONSTRUCTION TOLERANCE OF ±0.5% ALLOWED. OTHER CROSS-SLOPES MAY BE ALLOWED; HOWEVER, THE SUM OF THE FIELD ADJUSTED CROSS-SLOPE PLUS CONSTRUCTION TOLERANCE SHALL NOT EXCEED 2.0%.
 - GRADE AND CONSTRUCT ALL SIDEWALKS AND WALKS, REGARDLESS OF MATERIAL, WITH A MAXIMUM TRANSVERSE SLOPE OR RUNNING SLOPE SHALL NOT EXCEED 4% WITH A CONSTRUCTION TOLERANCE OF ±1% ALLOWED. OTHER RUNNING SLOPES MAY BE ALLOWED; HOWEVER, THE SUM OF THE FIELD ADJUSTED RUNNING SLOPE PLUS CONSTRUCTION TOLERANCE SHALL NOT EXCEED 5%.
- IF A SIDEWALK OR WALK IS LOCATED IN THE PUBLIC RIGHT-OF-WAY, THE RUNNING SLOPE OF THE SIDEWALK MAY FOLLOW THE ADJACENT ROADWAY RUNNING SLOPE, BUT CANNOT EXCEED THE GENERAL GRADE OF THE ADJACENT ROADWAY.
- THE IDENTITY AND LOCATION OF THE EXISTING UNDERGROUND UTILITIES KNOWN TO BE LOCATED WITHIN THE PROJECT LIMITS ARE SHOWN ON THE PLANS BASED ON THE BEST AVAILABLE INFORMATION AT THE TIME OF DESIGN.
 - NO EXTRA COMPENSATION WILL BE PAID TO THE CONTRACTOR BY REASON OF COMPLIANCE WITH ANY OF THE REQUIREMENTS INDICATED ON THE PLANS, BUT PAYMENT SHALL BE DEEMED TO BE INCLUDED IN THE PROPOSAL ITEMS, AS BID UPON, UNLESS OTHERWISE SPECIFICALLY PROVIDED.
 - CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THEIR OWN HORIZONTAL AND VERTICAL CONTROL POINTS, BENCHMARKS, ETC. CONTRACTOR IS RESPONSIBLE FOR PROVIDING CONSTRUCTION STAKING AND FIELD LAYOUT.



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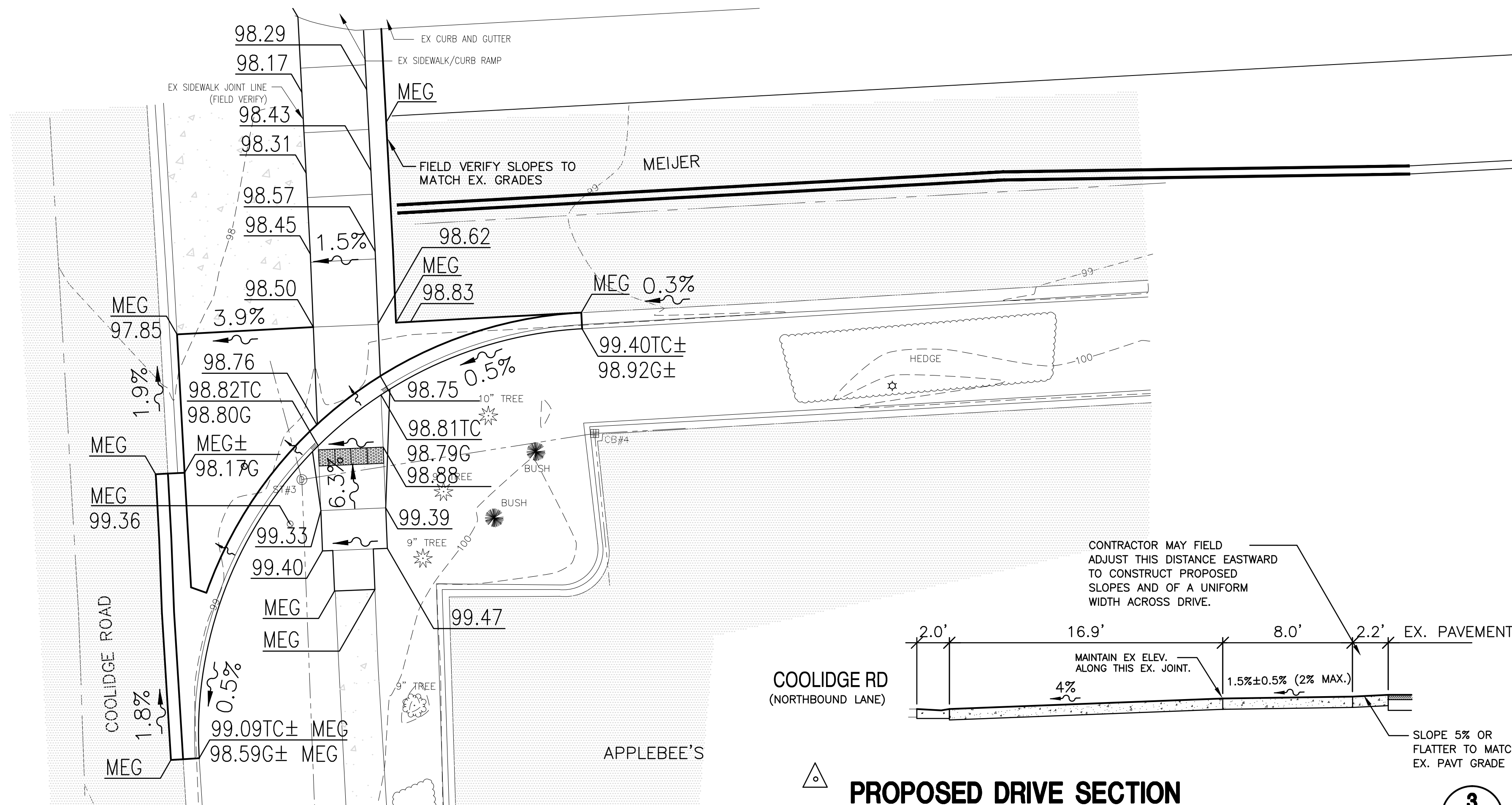


LEGEND

	EXISTING CONTOUR ELEVATION
	PROPOSED MATCH EX. GRADE (FINISH GRADE)
	PROPOSED SPOT GRADE (FINISH GRADE)
	PROP TOP OF CURB GRADE (FINISH GRADE)
	PROPOSED GUTTER GRADE (FINISH GRADE)
	PROPOSED DIRECTION OF SURFACE RUN-OFF

EXISTING STRUCTURE INVENTORY

- STORM MANHOLE #1**
RIM - 100.51
20" RCP NORTH - 94.25
15" RCP SOUTH - 94.34
12" RCP S30°W - 94.69
- CATCH BASIN #2**
INLET - 99.42
12" RCP N30°E - 95.10
12" RCP WEST - 95.17
- STORM MANHOLE #3**
RIM - 99.29
24" RCP N15°W - 93.14
14" PVC EAST - 93.39
20" RCP SOUTH - 93.94
- CATCH BASIN #4**
INLET - 99.18
14" PVC WEST - 93.69
12" PVC EAST - 94.13
12" PVC SOUTH - 93.90



LEGEND

	ASPHALT
	CONCRETE
	EXISTING SPOT ELEVATION
	EXISTING CONTOUR ELEVATION
	SANITARY SEWER
	STORM SEWER
	WATER LINE
	GAS LINE
	SANITARY MANHOLE
	DRAINAGE MANHOLE
	ELECTRIC MANHOLE
	TELEPHONE MANHOLE
	CATCHBASIN
	CLEANOUT
	FIRE HYDRANT
	VALVE
	UTILITY POLE
	UTILITY PEDESTAL
	DECIDUOUS TREE
	CONIFEROUS TREE

BENCHMARKS

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CHISELED SQUARE ON SOUTH SIDE OF CONCRETE LIGHT POLE BASE IN NORTHWEST QUAD OF NORTH DRIVE TO KROGER OFF COOLIDGE ROAD
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TOP OF BIG ARM OF FIRE HYDRANT SOUTH SIDE OF DRIVE BETWEEN BANK OF AMERICA AND APPLEBEES ON EAST SIDE OF COOLIDGE ROAD

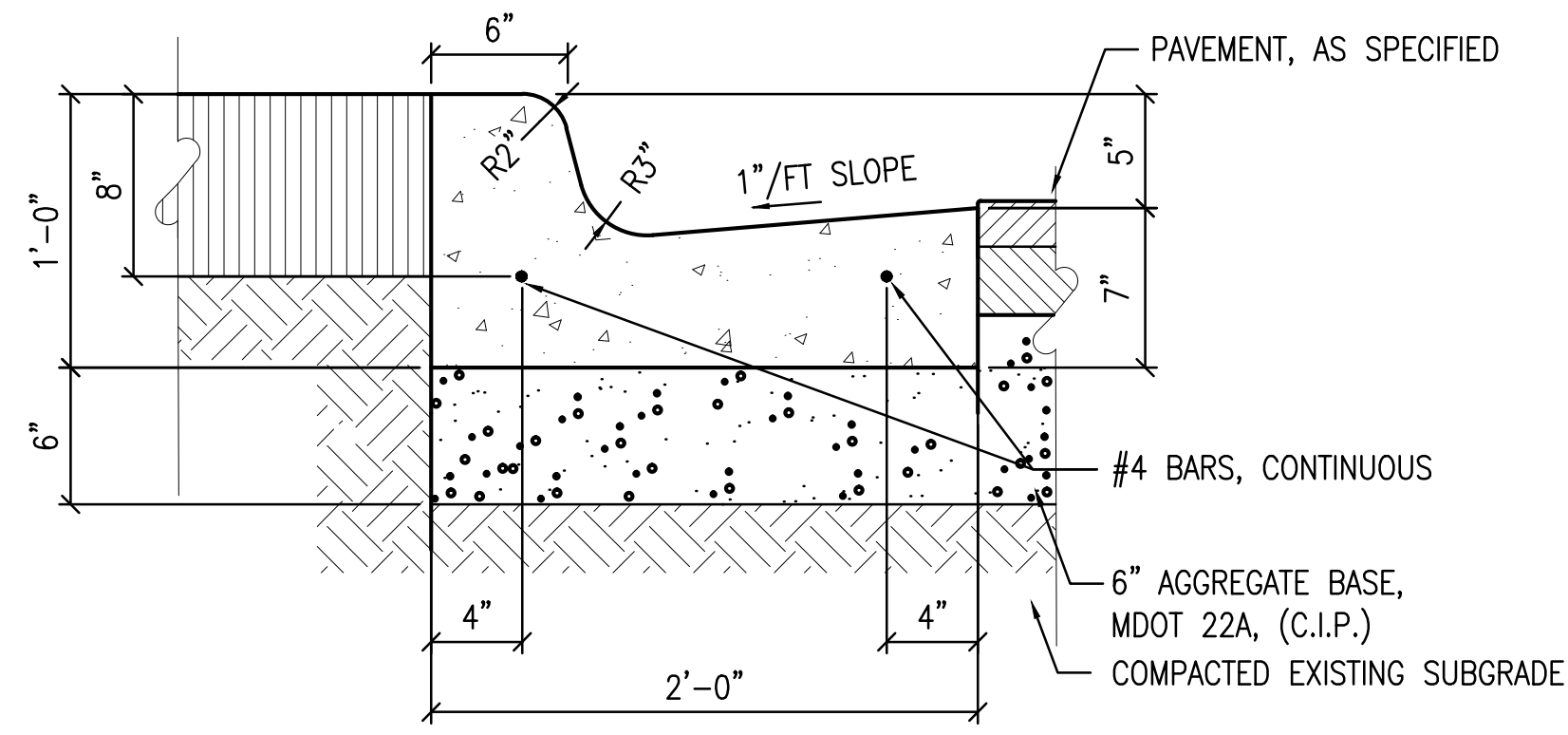


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DRAWN: RMS
DESIGNED: RMS
APPROV'D: SOM
DATE: JUNE 26, 2012
PROJECT NUMBER
0941-6302-00

MICHIGAN
EAST LANSING
CAPITAL AREA TRANSPORTATION AUTHORITY
MODIFICATION TO EX. BOARDING CENTER AT LAKE LANSING MEIJER
PROPOSED IMPROVEMENTS & GRADING PLAN

DRAWING NUMBER
C2.1
SITE/CIVIL



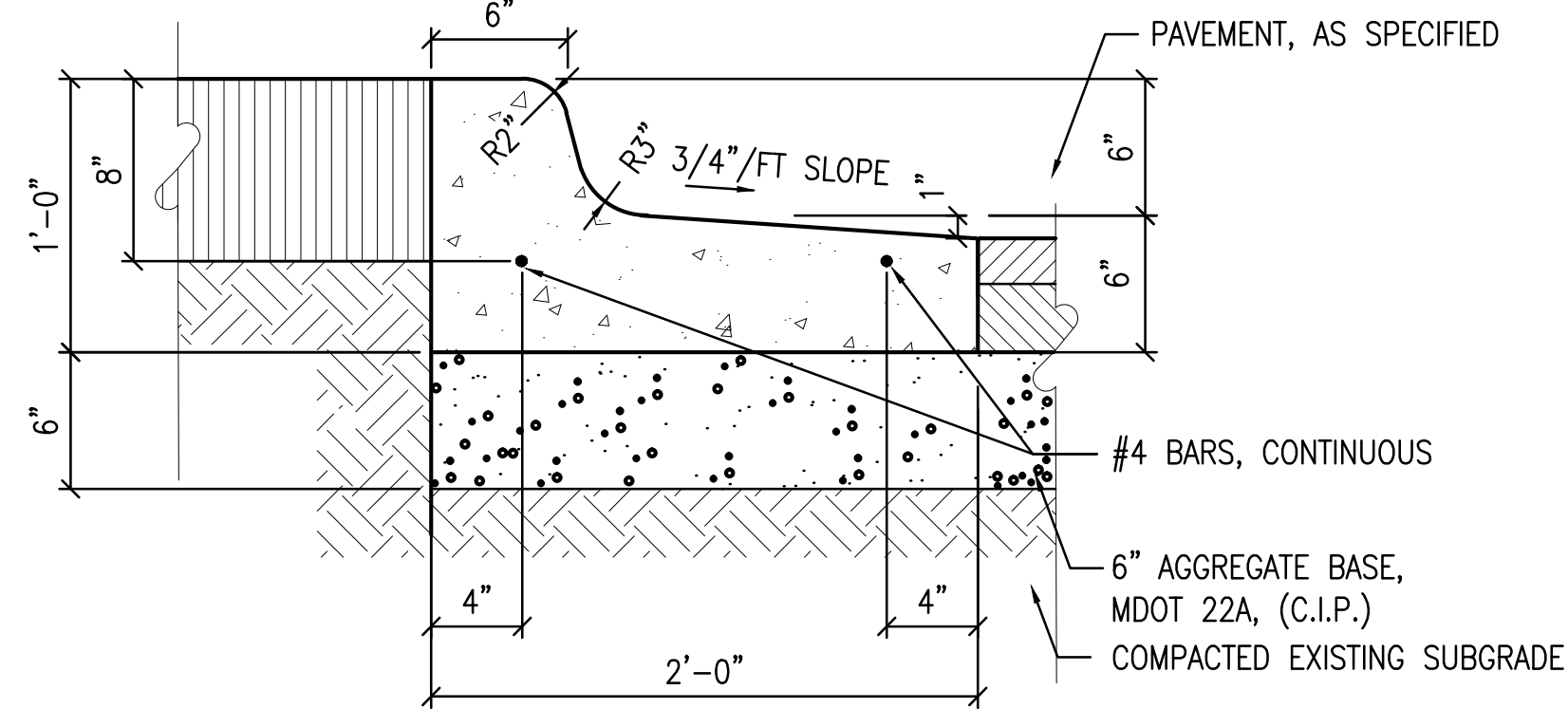
NOTE: PLACE CONTROL JOINTS 10' O.C. & ISOLATION JOINTS (1" FIBER) 400' O.C.

STANDARD CURB AND GUTTER SECTION

SCALE: 1-1/2"=1'-0"

1

C3.1



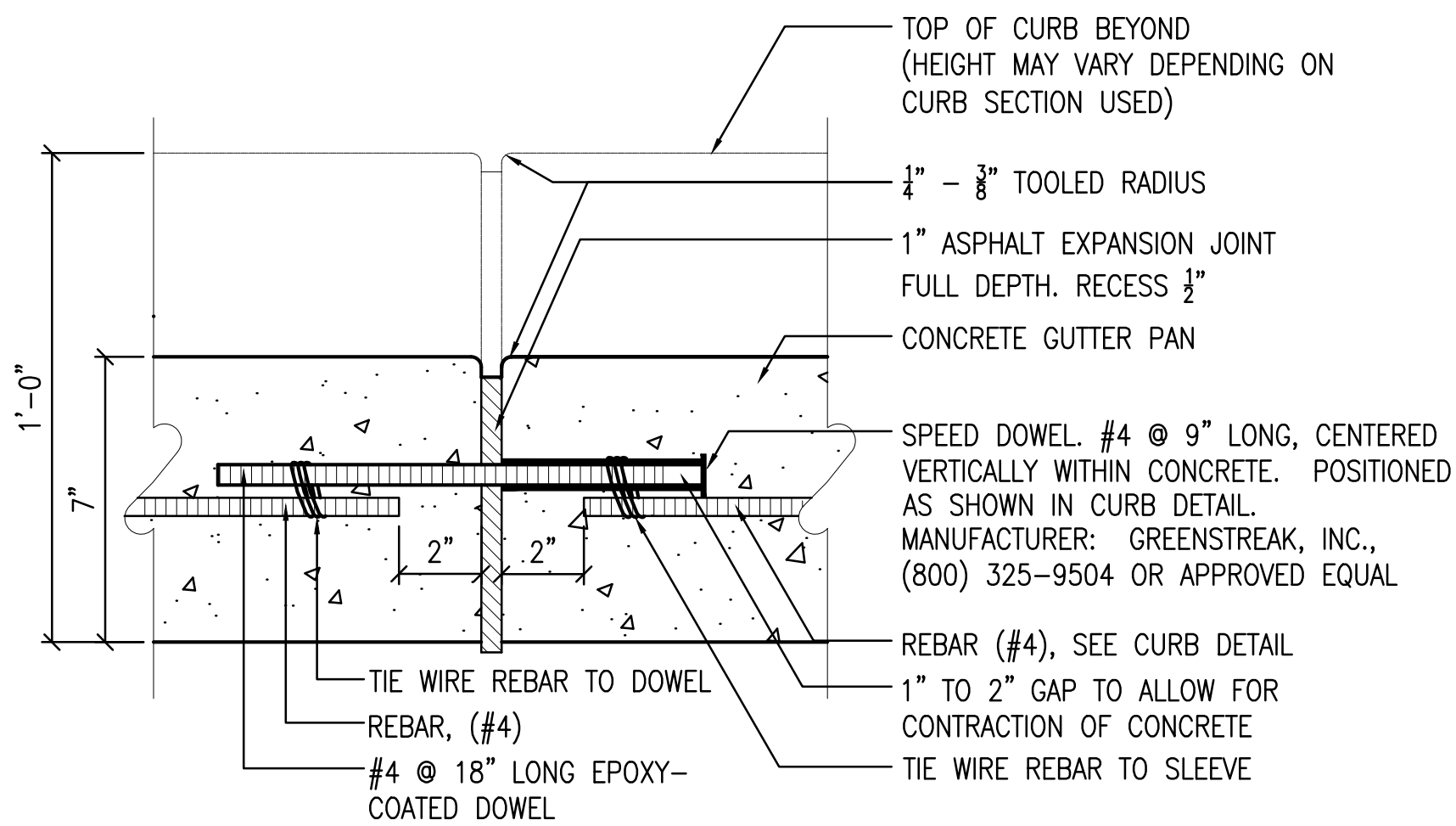
NOTE: PLACE CONTROL JOINTS 10' O.C. & ISOLATION JOINTS (1" FIBER) 400' O.C.

PITCH OUT CURB AND GUTTER SECTION

SCALE: 1-1/2"=1'-0"

2

C3.1

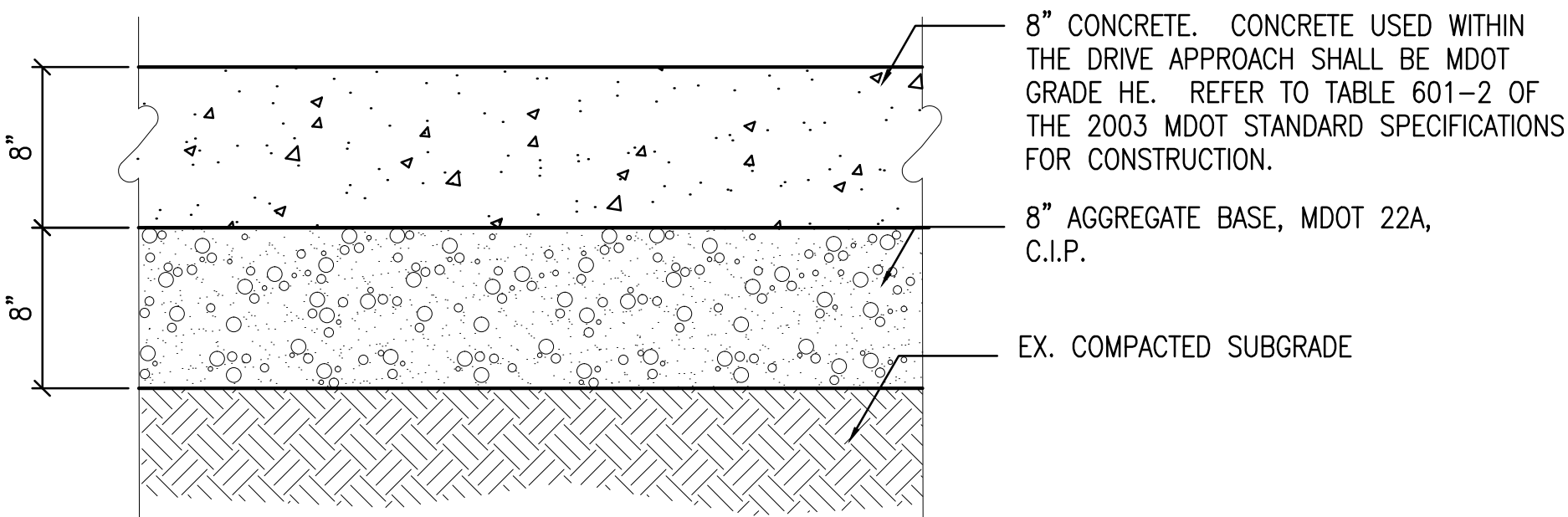


CURB EXPANSION JOINT

SCALE: 3"=1'-0"

5

C3.1



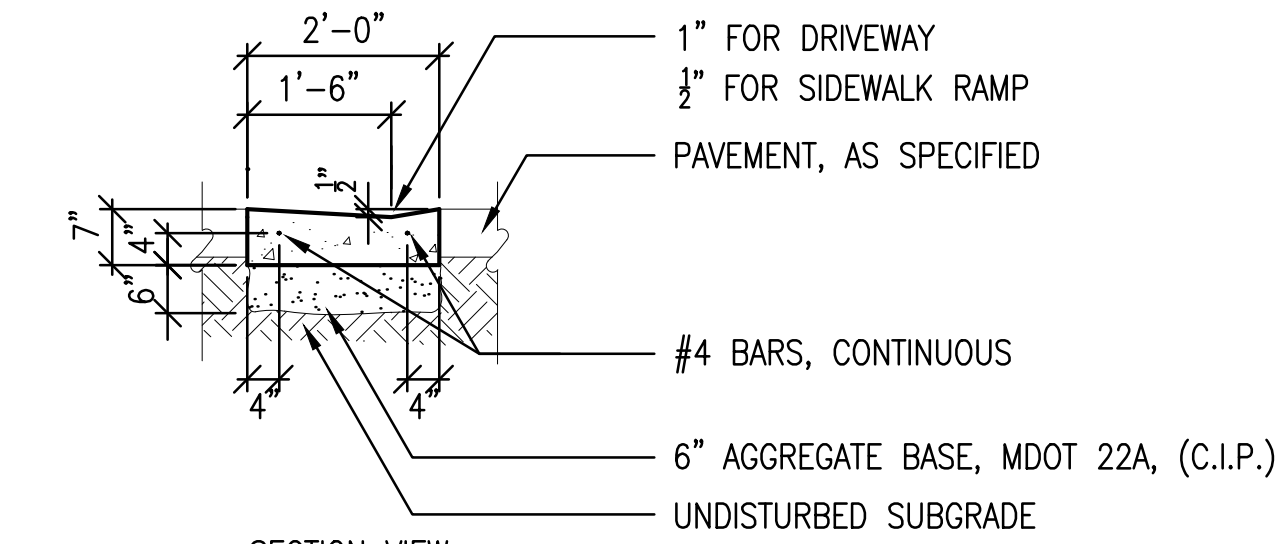
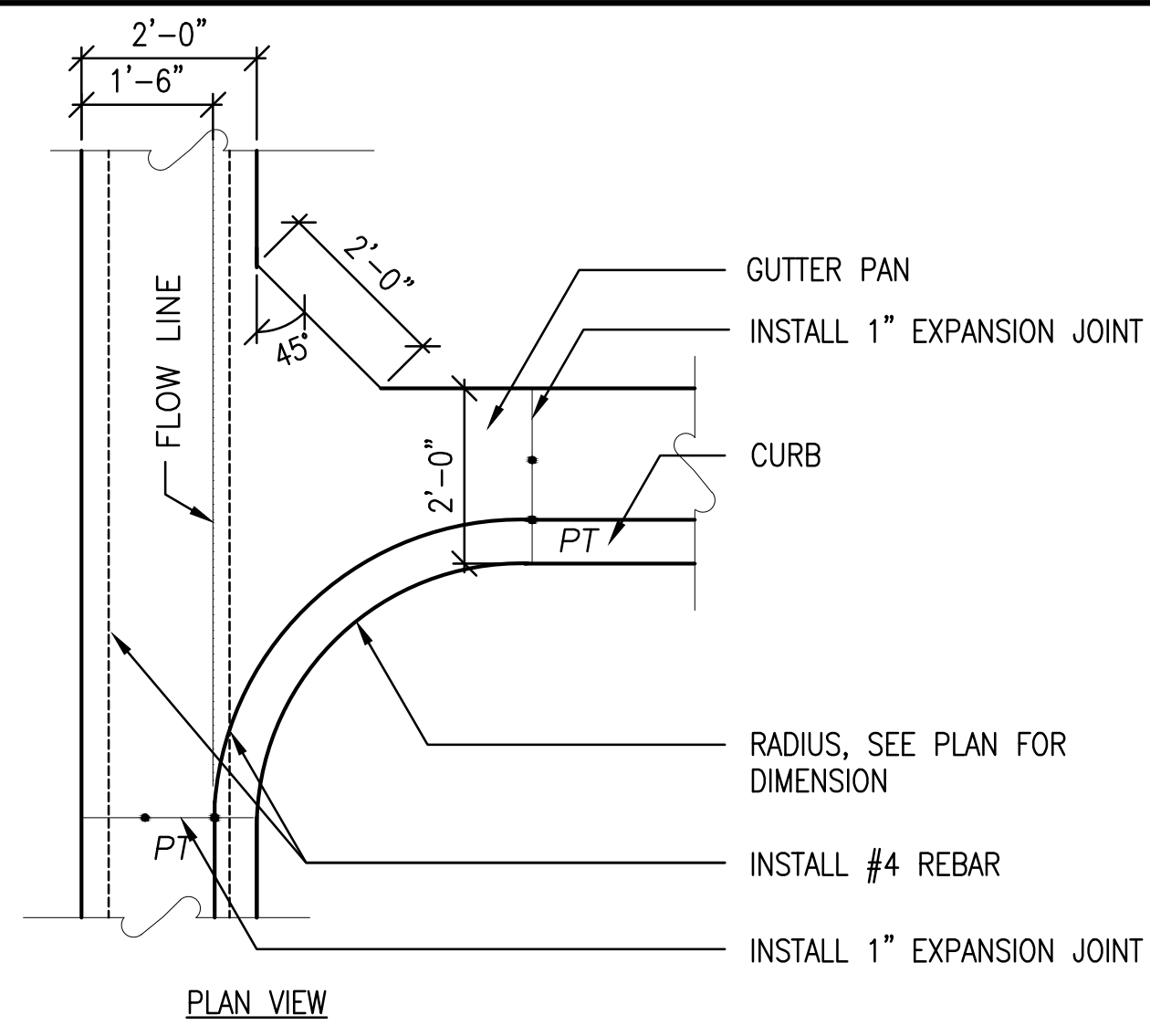
NOTE:
1. AT THE TIME OF CONSTRUCTION, CONTRACTOR SHALL FIELD VERIFY DEPTH OF EX. CONCRETE PAVEMENT.
2. REPLACE EX. DRIVE APPROACH CONCRETE WITH THE SAME THICKNESS AND TYPE AS THE EXISTING PAVEMENT ON GRAVEL (MDOT 22A) EQUAL TO THE EXISTING SUBBASE IN THICKNESS AND COMPACTED TO 98% DENSITY.

CONCRETE DRIVE APPROACH

SCALE: 1-1/2"=1'-0"

6

C3.1

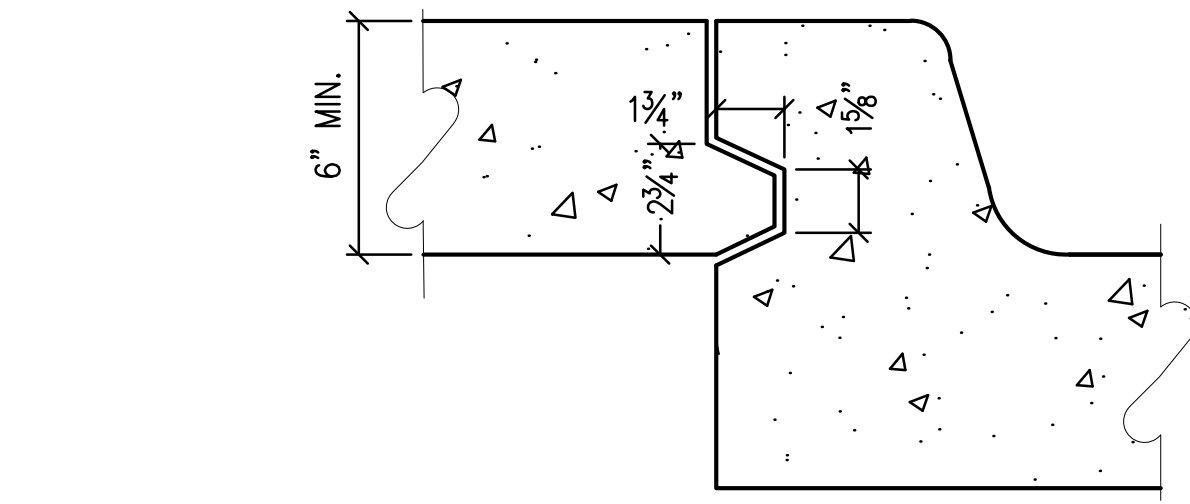


VALLEY GUTTER PLAN AND SECTION

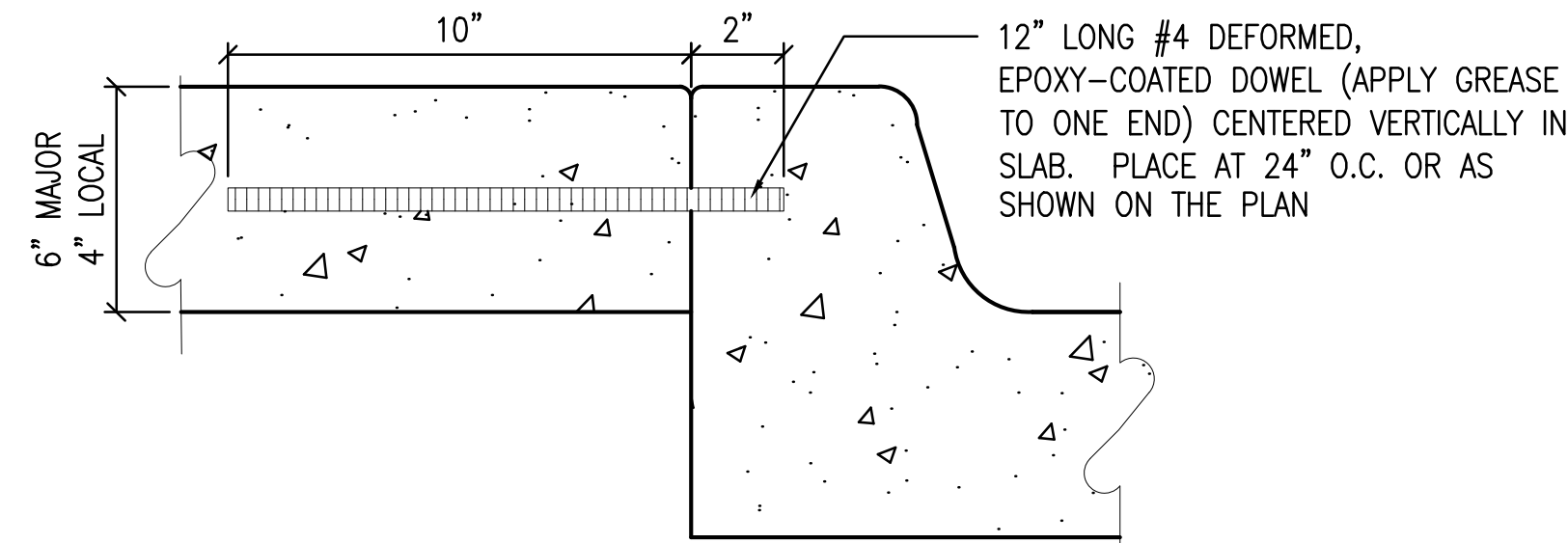
SCALE: 1/2"=1'-0"

3

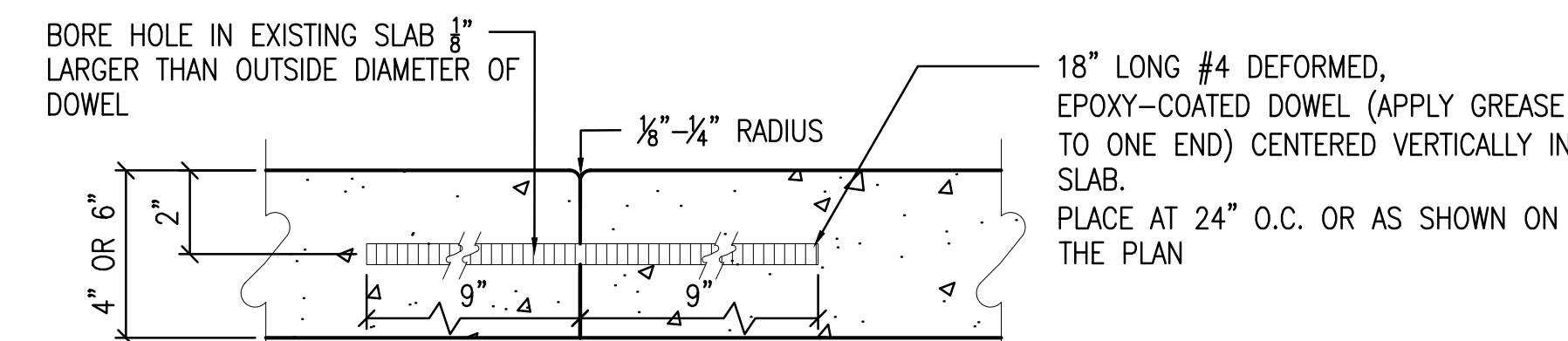
C3.1



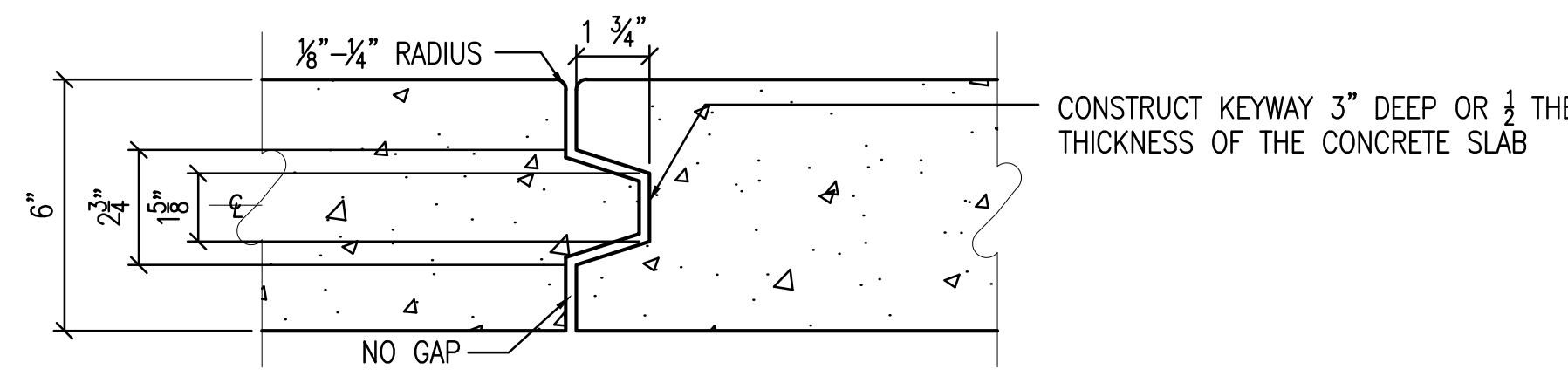
OPTION 1 - KEYWAY AT BACK OF CURB



OPTION 2 - DOWEL AT BACK OF CURB



OPTION 3 - DOWEL



OPTION 4 - KEYWAY

CONSTRUCTION JOINTS FOR 4'-6" SIDEWALK THICKNESS

SCALE: 3"=1'-0"

7

C3.1

CONCRETE SIDEWALKS, CURB AND GUTTER, AND DRIVE APPROACH SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF EAST LANSING STANDARD CONSTRUCTION SPECIFICATIONS AND DETAILS AND THE PROPOSED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT-OF-WAY (PROWAG).

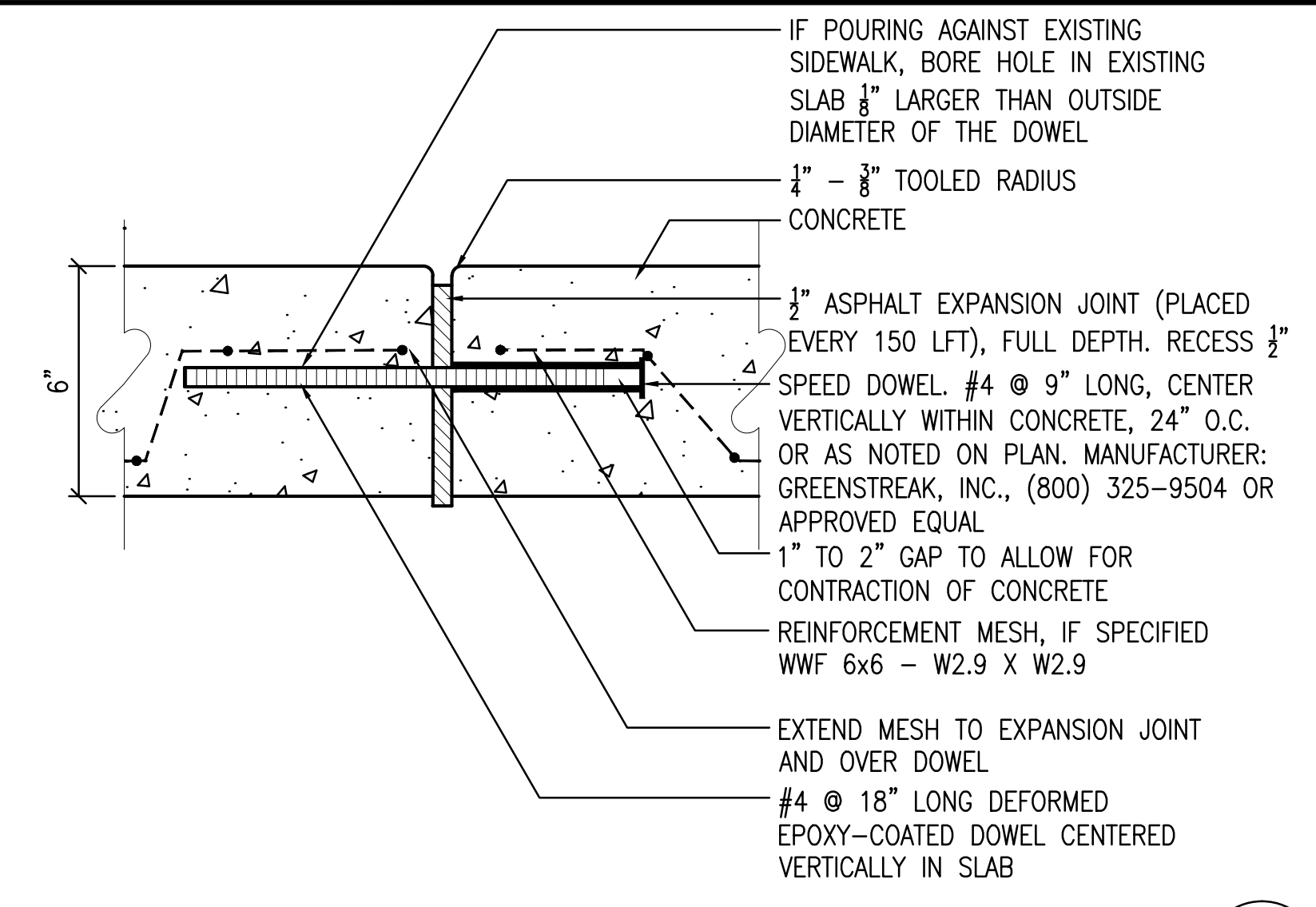
CONTRACTOR MAY ELECT TO USE MDOT P-NC TYPE MIX ONLY IN VEHICULAR AREAS TO EXPEDITE OPENING DRIVE APPROACH.

CONCRETE MIXTURE

- CONCRETE: 3,500 PSI MIN.
- CEMENT CONTENT: 6 SACK, MDOT P1
- COARSE AGGREGATE: LIMESTONE; GRADATION PER MDOT SPEC. 6AA
- FINE AGGREGATE: LIMESTONE OR OTHER GRADATION PER MDOT SPEC 2NS
- WATER/CEMENT RATIO: 0.46
- SLUMP: 4" MAX
- AIR CONTENT: 5% -8%
- FINISH: BROOMED FINISH
- CURE & SEAL: WHITE CURING COMPOUND AT A RATE OF 1 GAL/200 SFT.

CONCRETE MIXTURE

- CONCRETE: 3,500 PSI MIN.
- CEMENT CONTENT: 7 SACK, MDOT P-NC
- COARSE AGGREGATE: LIMESTONE; GRADATION PER MDOT SPEC. 6AA
- FINE AGGREGATE: LIMESTONE OR OTHER GRADATION PER MDOT SPEC 2NS
- WATER/CEMENT RATIO: 0.46
- SLUMP: 4" MAX
- AIR CONTENT: 5% -8%
- FINISH: BROOMED FINISH
- CURE & SEAL: WHITE CURING COMPOUND AT A RATE OF 1 GAL/200 SFT.

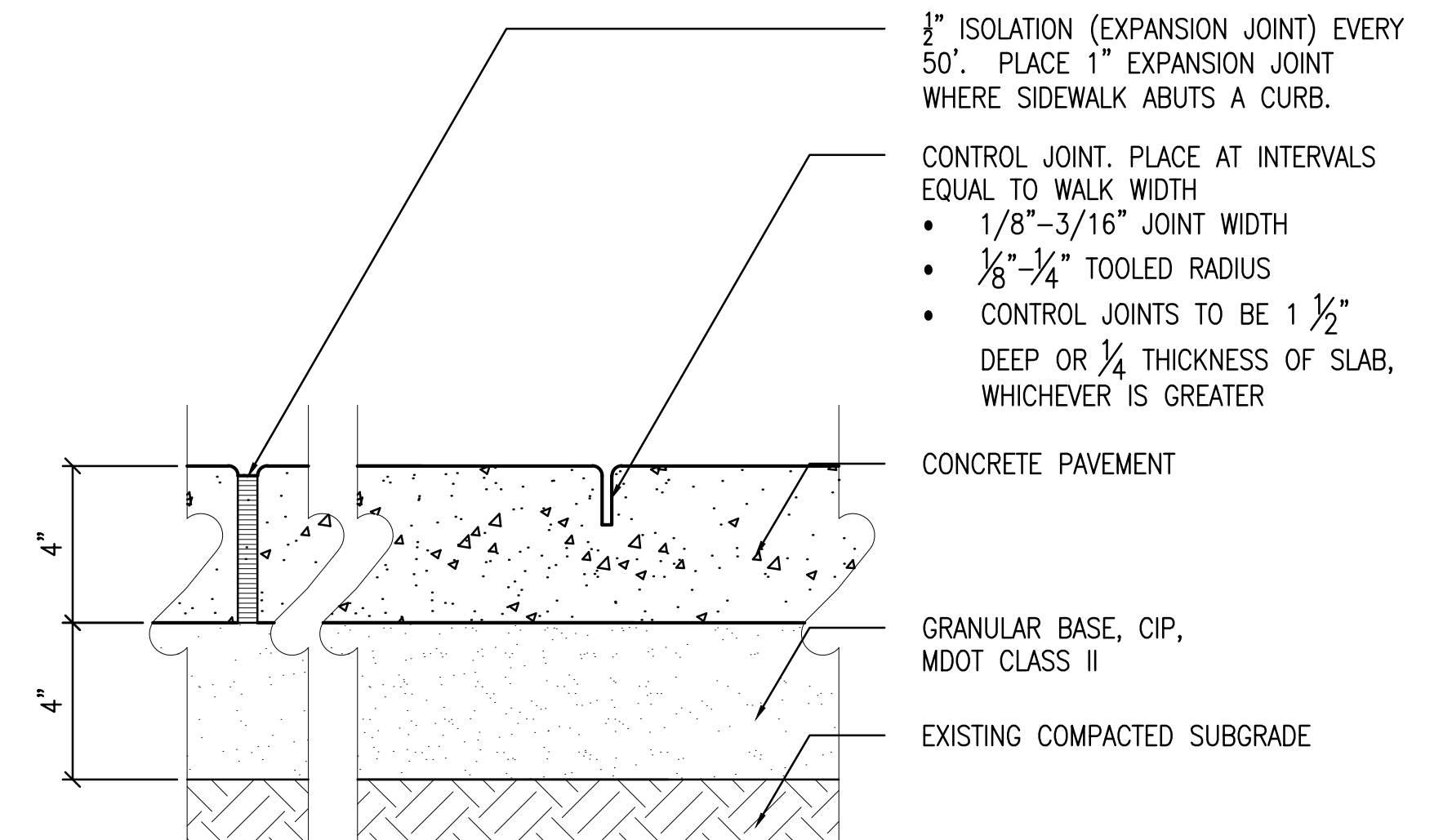


EXPANSION JOINT SECTION

SCALE: 3"=1'-0"

4

C3.1

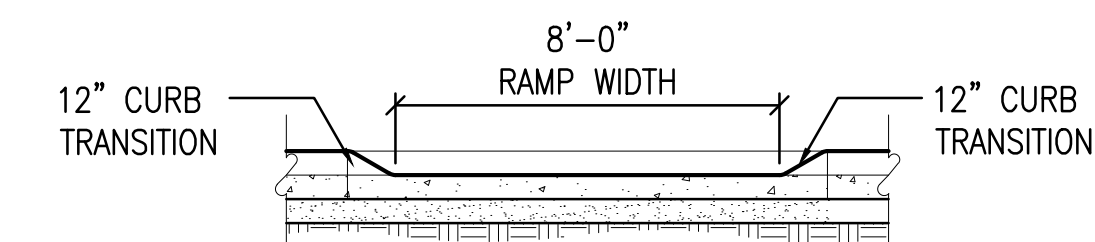


CONCRETE WALK AND JOINT

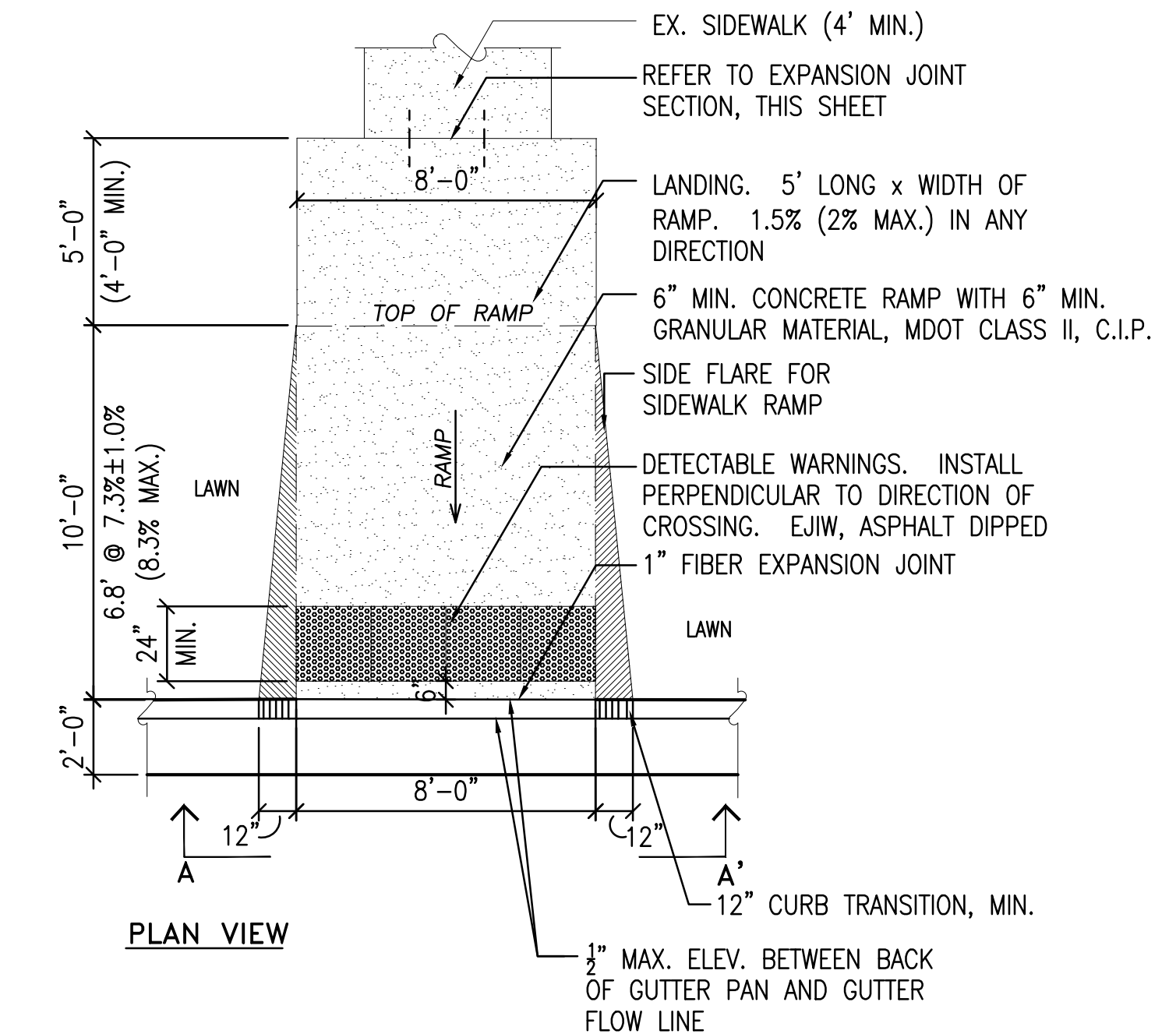
SCALE: 3"=1'-0"

8

C3.1



SECTION A-A'

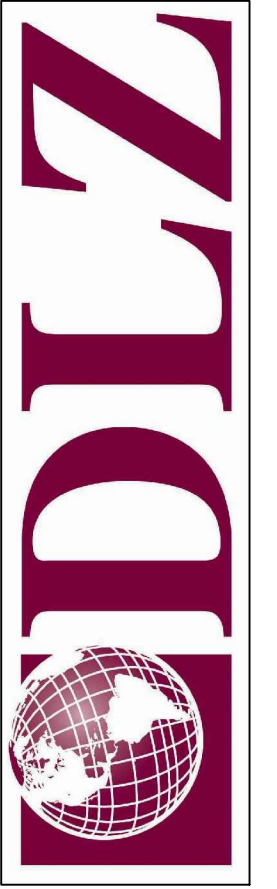


SIDEWALK RAMP, PERPENDICULAR

SCALE: 1/4"=1'-0"

9

C3.1



DLZ MICHIGAN, INC.

0941-6302-00

EAST LANSING
CAPITAL AREA TRANSPORTATION AUTHORITY
MODIFICATION TO EX. BOARDING CENTER AT LAKE LANSING MEIJER

CONSTRUCTION DETAILS

C3.1
SITE/CIVIL